

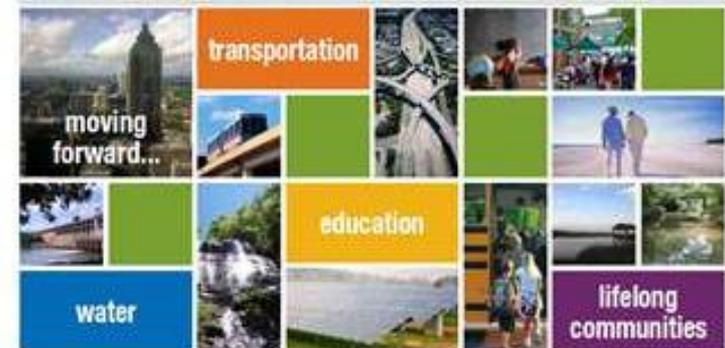
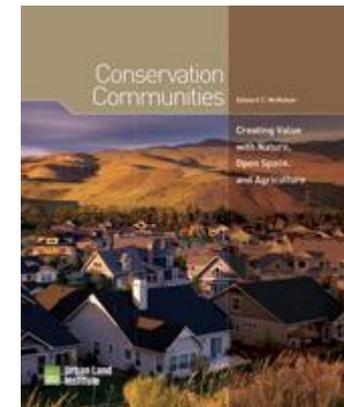


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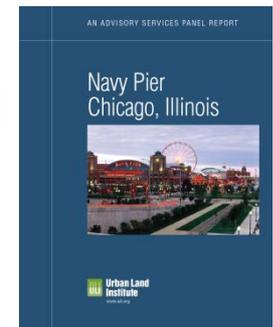
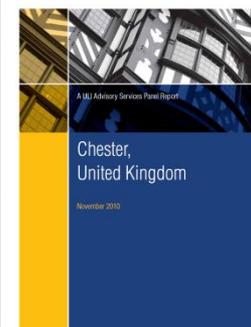
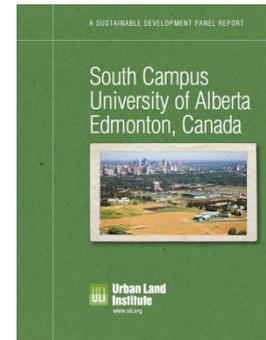
Advisory Services Program

Tampa, Florida · 10/9/11 – 10/14/11

- The mission of the Urban Land Institute is to provide leadership in the responsible use of land and in creating and sustaining thriving communities worldwide.
- ULI is a membership organization with nearly 30,000 members, worldwide representing the spectrum of real estate development, land use planning and financial disciplines, working in private enterprise and public service.
- What the Urban Land Institute does:
 - Conducts Research
 - Provides a forum for sharing of best practices
 - Writes, edits and publishes books and magazines
 - Organizes and conducts meetings
 - Directs outreach programs
 - Conduct Advisory Services Panels



- Since 1947
- 15 - 20 panels a year on a variety of land use subjects
- Provides independent, objective candid advice on important land use and real estate issues
- Process
 - Review background materials
 - Receive a sponsor presentation & tour
 - Conduct stakeholder interviews
 - Consider data, frame issues and write recommendations
 - Make presentation
 - Produce a final report



Our Panelists, who have been working diligently in your community this week:

- Tim Dreese - Matrix Design Group, Denver, CO
- David Scheuer – Retrovest Companies Burlington, VT
- Kelli Bernard – LADWP, Los Angeles, CA
- Ellen McCarthy – Arent Fox, LP, Washington, D.C.
- Ed Starkie – Urban Advisors Ltd., Portland OR
- Byron Koste – Univ. CO, Leeds School of Business, Centennial, CO
- Ralph Nuñez – Design Team, Southfield, MI



The City of Tampa

Special Thanks to:

Tampa Downtown Partnership

ULI Tampa Bay



“To outline a vision for the central city area and identify uses and opportunities for further and more detailed examination by the consultant team (and) ... to identify specific actions that can be taken immediately to stimulate economic development and promote a positive business climate.”



- Urban / Neighborhood Vision
- Redevelopment Issues
- Transportation/Connectivity
- Market/ Market Potential
- Organizational Structure/Issues
- Implementation
 - Short Term
 - Long Term
- Financing Alternatives

What are the possibilities for this study area in becoming a true urban center?



What are the possibilities for this study area in becoming a true urban center? What are seen as the key obstacles to overcome and the key opportunities to capitalize on these possibilities?

A true urban center needs to have:

- A “soul”
- A variety of
 - Gathering places
 - Comfortable parks
 - Urban plazas
 - Public spaces



What are the possibilities for this study area in becoming a true urban center?

A true urban center needs to have:

- A strong, diverse, economic base
- A variety of land use types and densities



A true urban center needs to have:

- A mix of higher density housing types
- Close-in walkable neighborhoods
- An effective, pedestrian friendly transportation system



A true urban center needs to have:

- Sense of permanence
- Visionary leadership



What are the best short and long term development opportunities?



What are the best short and long term development opportunities?

Office

- The short term demand for new downtown high rise office is limited
- Current conditions do not suggest significant growth



Hotel

- The City should welcome a 5-Star hotel in the downtown
- Ybor City would benefit from additional hotel capacity



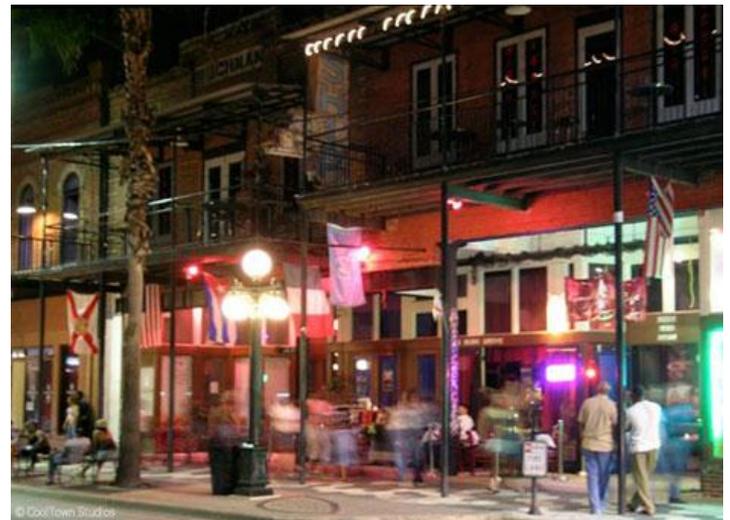
Commercial

- Potentially deep market of start-up businesses
- The City should undertake a targeted marketing plan directed at identifying and supporting emerging entrepreneurs
- Provide real estate “clearing house services”



Retail

- Retail in the CBD is fragmented and stressed with high vacancies, deteriorating store fronts
- The City should develop targeted strategy to support retail and restaurant growth.
 - Plan to support retail in a few concentrated areas in the CBD where double-loaded retail can be successful on a block-by-block basis
 - Ybor City has asked for help in improving public safety.



Housing

- Tampa is expected to grow by 40,000 households by 2025
- Capturing this growth downtown is critical to the future of the city
- New residential units will:
 - Enliven and enrich the downtown
 - Support retail and restaurants
 - Provide walkable, near-in housing opportunities for downtown workers
 - Support future public transit
 - Improve the downtown tax base



Housing

- Inventory site opportunities
- Develop an aggressive program of disposition
- Implement the recommendations of the 2008 AIA Sustainability Report
 - Moratorium on further private surface lots
 - Convert public surface lots to parks and housing
 - Ban parking lots from street corners
 - Increase on-street parking
 - Improve landscaping and streetscapes



Housing

- The Channel District is an excellent and stable model
- The Encore Project is ambitious and critical
 - Questions raised about long-term build-out
- Northern Boulevard Homes
 - Target for appropriate redevelopment
- Student and employee housing
 - University of Tampa
 - Other educational institutions
 - Tampa General



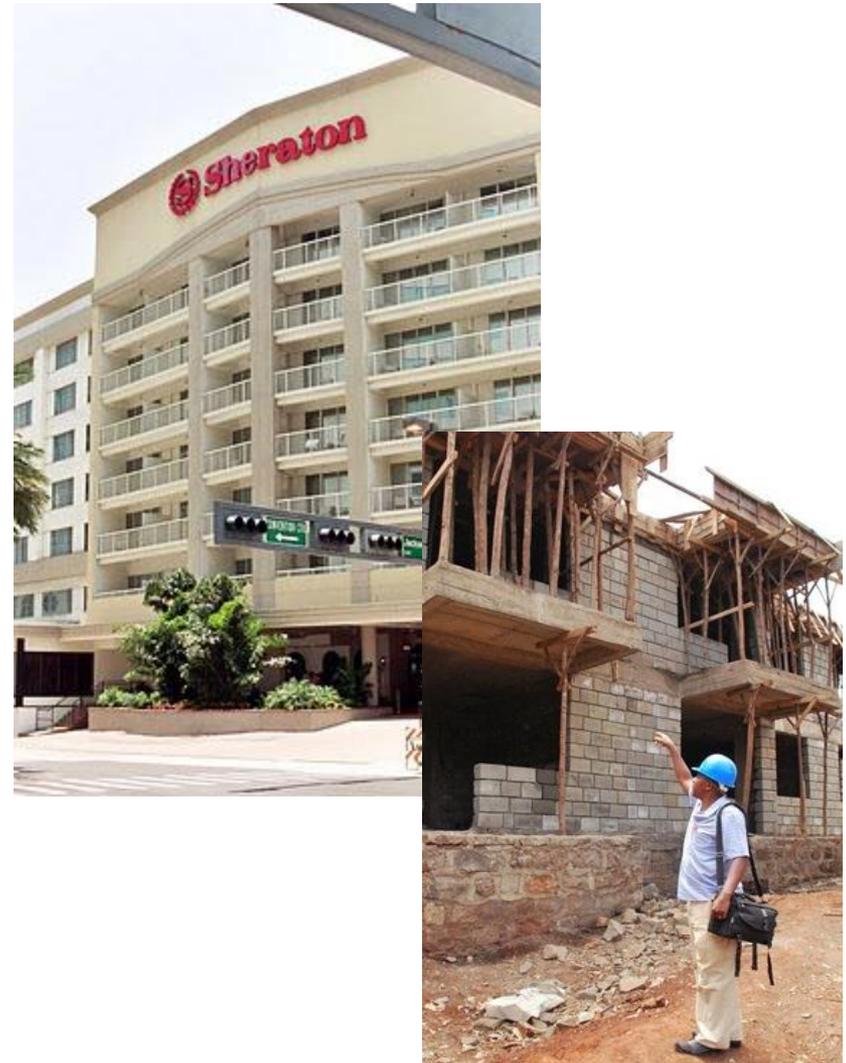
Economic Development

- Development opportunities provide economic opportunities for a cross section of Tampa's residents including students, young professionals, the large immigrant population and low skilled workers.
- A business incubator will support emerging entrepreneurs while providing new jobs for Tampa's highly skilled & educated workforce.



What are the best short and long term development opportunities?

New hotel and residential development will provide job creation and workforce training opportunities for those in the construction trades and service industry.



What are the best short and long term development opportunities?

- Working with the healthcare industry provides an opportunity for workforce housing and training.
- In Los Angeles, a collaboration of community college professionals, healthcare institutions and workforce development professionals developed an employer-driven training program to prepare candidates for successful careers in healthcare.

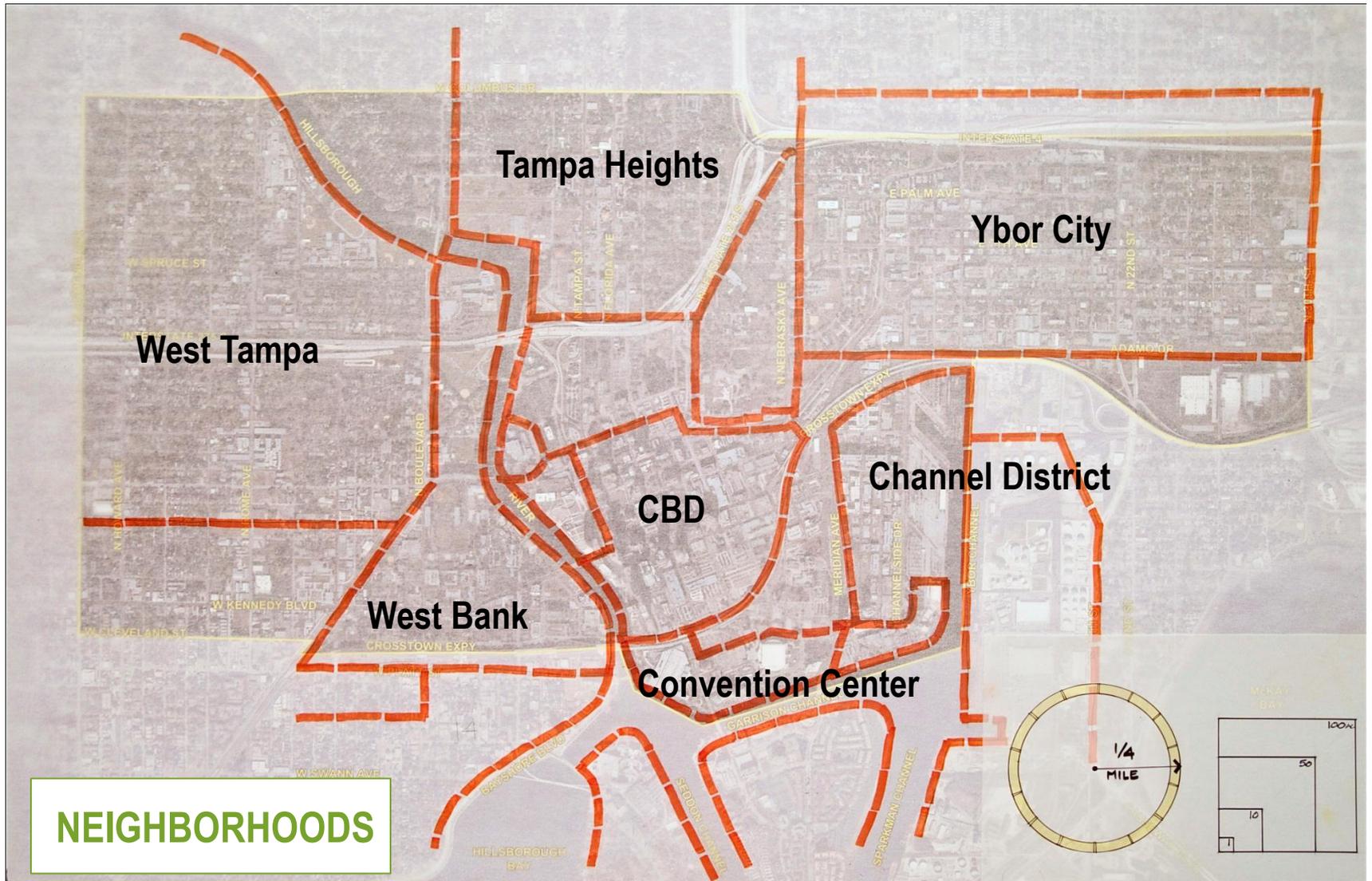


What are the best short and long term development opportunities?



Given the current market forecast and the various development opportunities in the study area, what are the best development opportunities in the short and long-term?

Where should the City focus its redevelopment efforts in the next 5 years?



Where should the City focus its redevelopment efforts in the next 5 years?



Channel District

- Expedite permit processing
- Reduce parking requirements
- Improve TECO trolley
- Encourage car-sharing firm(s)
- Develop safe bike paths
- Resolve Channelside Bay Plaza/Shops issue

Where should the City focus its redevelopment efforts in the next 5 years?



Riverwalk/North Franklin

- Recruit/encourage additional retail/entertainment venues
- Promote traffic calming
- Eliminate prohibition on sidewalk cafes/awnings on state-controlled arterials

Where should the City focus its redevelopment efforts in the next 5 years?



West of the River

- Redevelop North Boulevard Homes into mixed-income community
- Improve public land at Blake High School and Lane Park

Where should the City focus its redevelopment efforts in the next 5 years?



Encore Project

- Expedite development of Encore Project
- Re-examine assumptions regarding height and non-residential elements

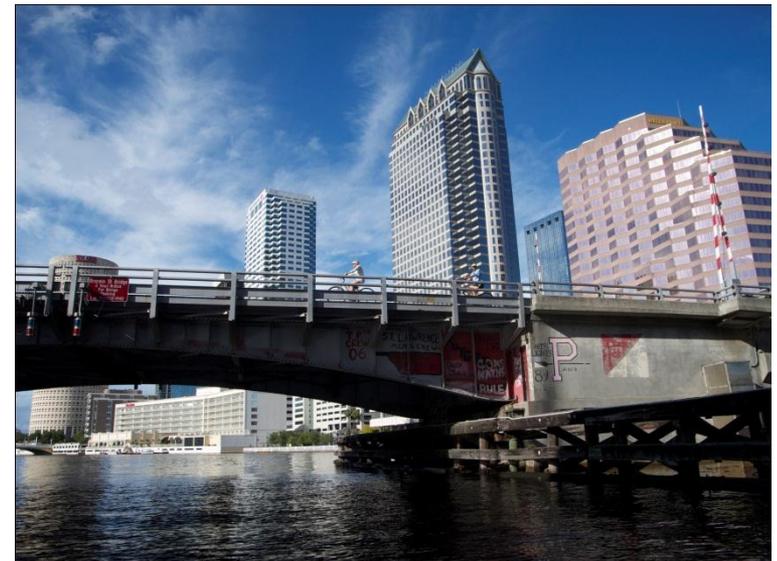
What are the redevelopment opportunities along both sides of the Hillsborough River?



What are the redevelopment opportunities along both sides of the Hillsborough River and how can the city maximize the development potential, connectivity, and amenity of the Hillsborough River in the study area?

What are the redevelopment opportunities along both sides of the Hillsborough River?

- Complete the Riverwalk on both sides of the River
- Utilize some or all of existing vehicular bridges to develop pedestrian crossing opportunities
- Remove/modify city regulation



What are the redevelopment opportunities along both sides of the Hillsborough River?

- Change the character and vehicular dominance of Ashley Drive
- Expand opportunities for marina-related activities
- Expand public events and activities on and adjacent to the river



What are the redevelopment opportunities along both sides of the Hillsborough River?

- Examine the use of all underutilized properties along both sides of the river
- Investigate EPA Brownfields funding for cleanup and redevelopment
- Partner with and engage the University of Tampa and other institutions



What development incentives will work best to facilitate desired development?



What development incentives will work best to facilitate desired development?

The City currently offers and markets a number of economic development incentives to encourage job creation, business growth & expansion and the removal of blight/redevelopment including:

- Federal Brownfield Redevelopment Bonus
- Florida State Enterprise Zone
- Hillsborough County Small Business Job Creation Program
- City of Tampa Transportation Impact Fee Exemption & Reduction

Other incentives Tampa may wish to consider include:

- Site-specific tax rebates
- Section 108 CDBG Loan Program
- Wage & Equipment credits that provide employee hiring tax credits and accelerated depreciation deductions



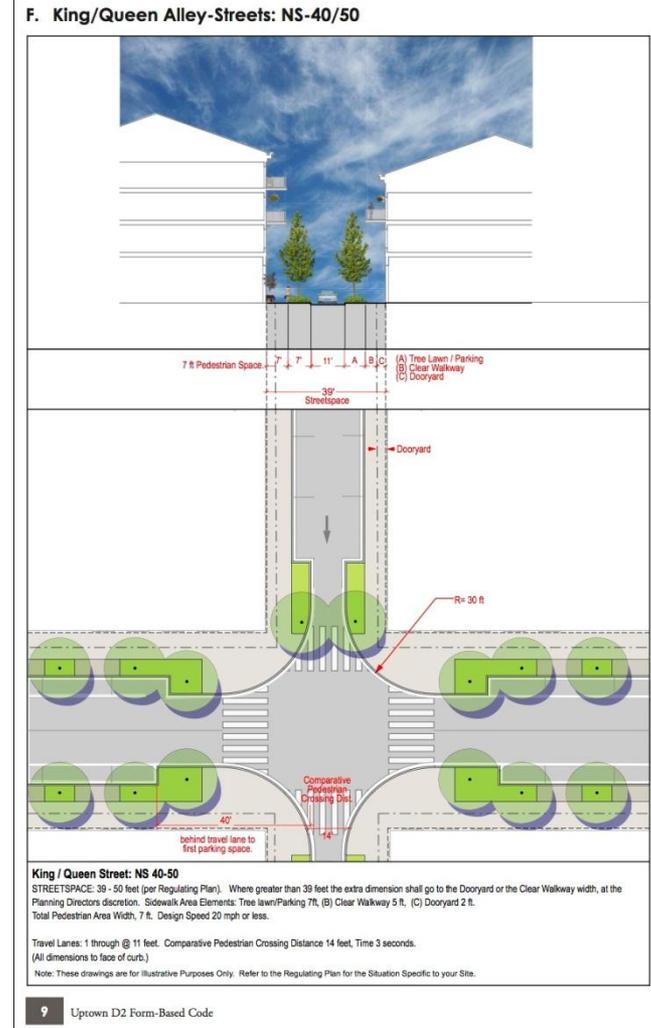
How does the city's efforts to transition to form-based regulations compare with best practices in other urban areas?



How does the city's efforts to transition to form-based regulations compare with best practices in other urban areas? Does this approach support the redevelopment of the study area?

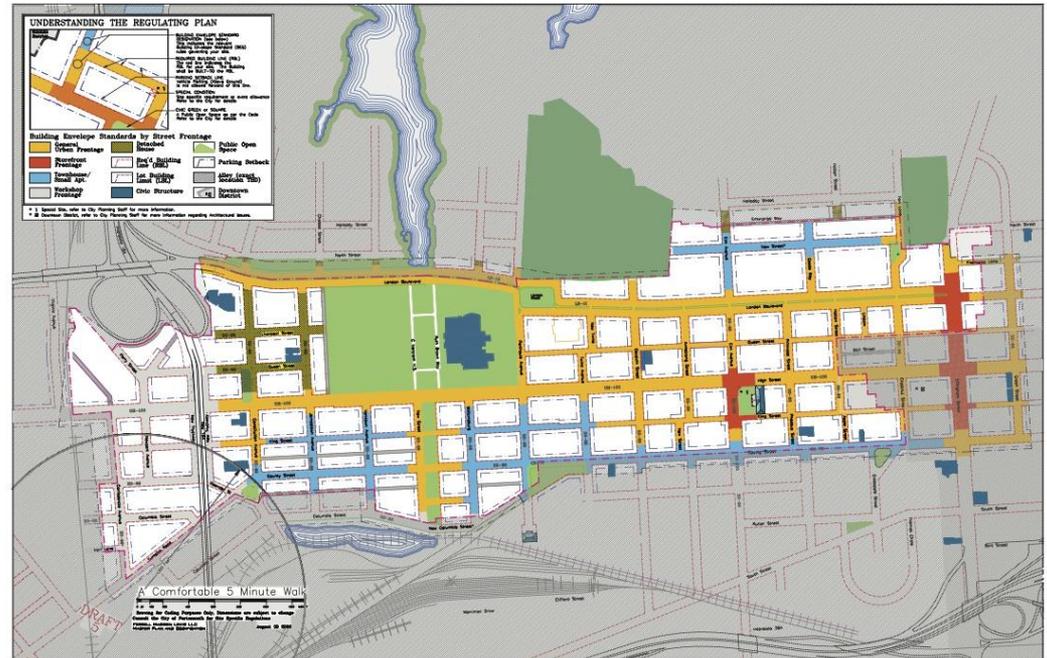
Form-based Zoning Codes

- What is form-based zoning?
- Nationwide there are hundreds of codes completed or in process
- Offers market flexibility
- Allows the exact coding of a public vision without the rewriting of an entire existing code



The Process

- Mutually agreed upon community vision
- Coding the relationships in the built environment
- What is built reflects what was envisioned.
- Code will differ from street to street, district to district or even parcel-to-parcel
- Code created that becomes an overlay to the existing code



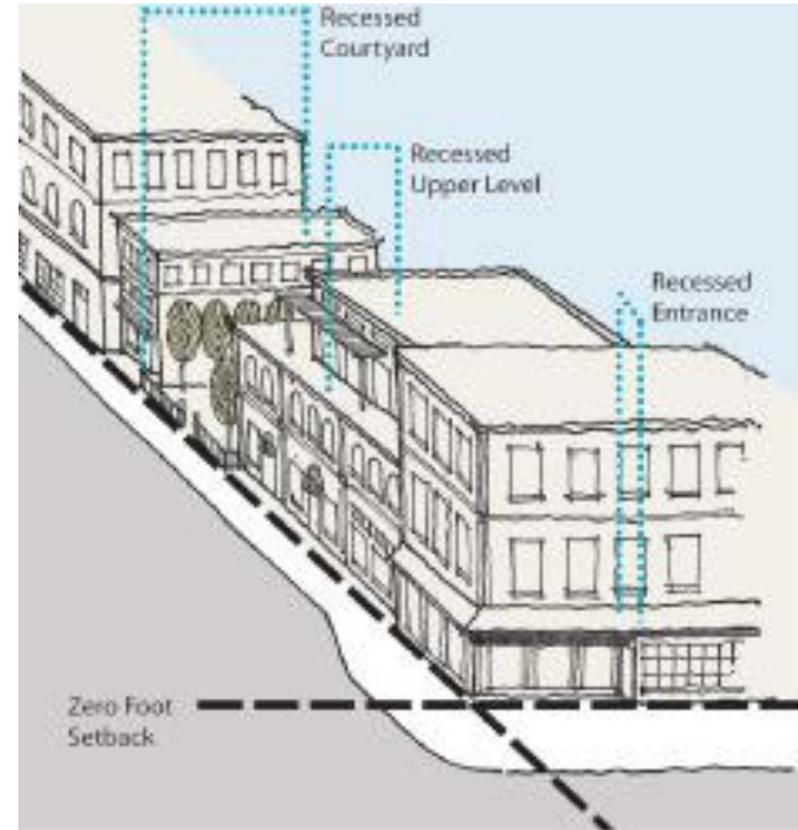
For the code to be effective

- Essential public agree upon the vision created
- Controversy is settled in the planning
- Development that adheres to the plan requirements can be carried out at an administrative level
- Saves the public sector, local developers and businesses time and money
- Not a panacea by itself. Without a clear master plan agreed to by the public it will fail



Advantage of form-based zoning codes

- Based upon a fully articulated vision
- Offers clarity regarding the desired urban form
- Can be used by each division of the city to form a single playbook



How can the city encourage TOD with bus service as the primary transit node?



How best can the city encourage transit-oriented development given that for the foreseeable future the primary mode of transit will be bus service?

Complete Walkable Transit-Ready Districts

- A different solution from auto-only orientation
- Not new
- Offers multiple modes of customer capture
- Allows seniors to age in place and young people to live in the biking, walking communities



The basic difference in the two models.

- Auto-only
- Large amounts of parking, and large streets with high traffic volumes



- Transit-ready sites can increase the building density and the leasable square footage
- Street widths can be smaller and more businesses can be financed and operated with little or no parking
- The Gap, Levi's Store, Crate and Barrel and others have built outlets in such urban districts
- Phasing is important

An economic development tool

- Transit becomes efficient because of the density
- Land use intensity lowers the cost of infrastructure per capita and therefore the cost of service per capita
- Increase the ability of the CBD to compete
- Commuting cost advantage in time and money to every employee who lives and works on the transportation network



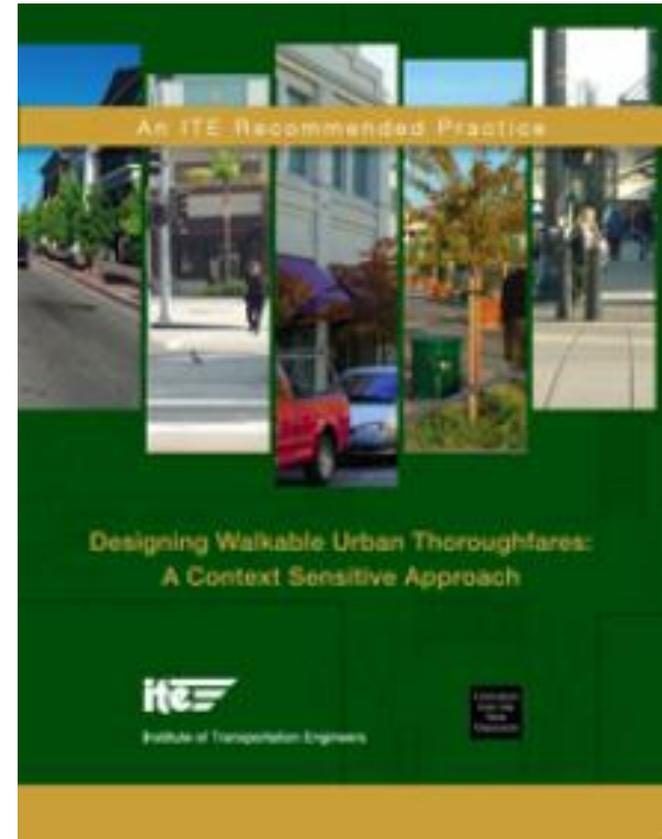
Complete Streets

- North Boulevard, Palm Avenue, and Bayshore Boulevard, and has completed improvements on Euclid Avenue
- Program should be incorporated in all new development
- Existing corridors like Ashley Drive, Florida Avenue, and Tampa Street



Complete Streets

- Offer a competitive economic advantage
- Streets could become economic generators for the core districts
- City should evaluate other large corridors according to the standards presented in the ITE Walkable Thoroughfares manual, and use this manual as a best practice guide for future rights of way



Low-Cost Transit Actions

- Improve frequency and duration of trolley service
- Create circulator service from southern terminus of trolley north on Franklin – short term
- Extend trolley tracks – within 5 years



Performance Parking

- Improve demand management of parking meters to improve the efficiency of use
- Set rate to achieve 85% utilization
- Convert meters to accept credit cards and/or mobile phone charges



Low-cost transit improvements

- Use cell phones to know when next bus arrives
- Improve bus signage
- Promote transit to conventioners and hotel guests
- Create fare-free zone
- Continue to pursue long-range transit improvements

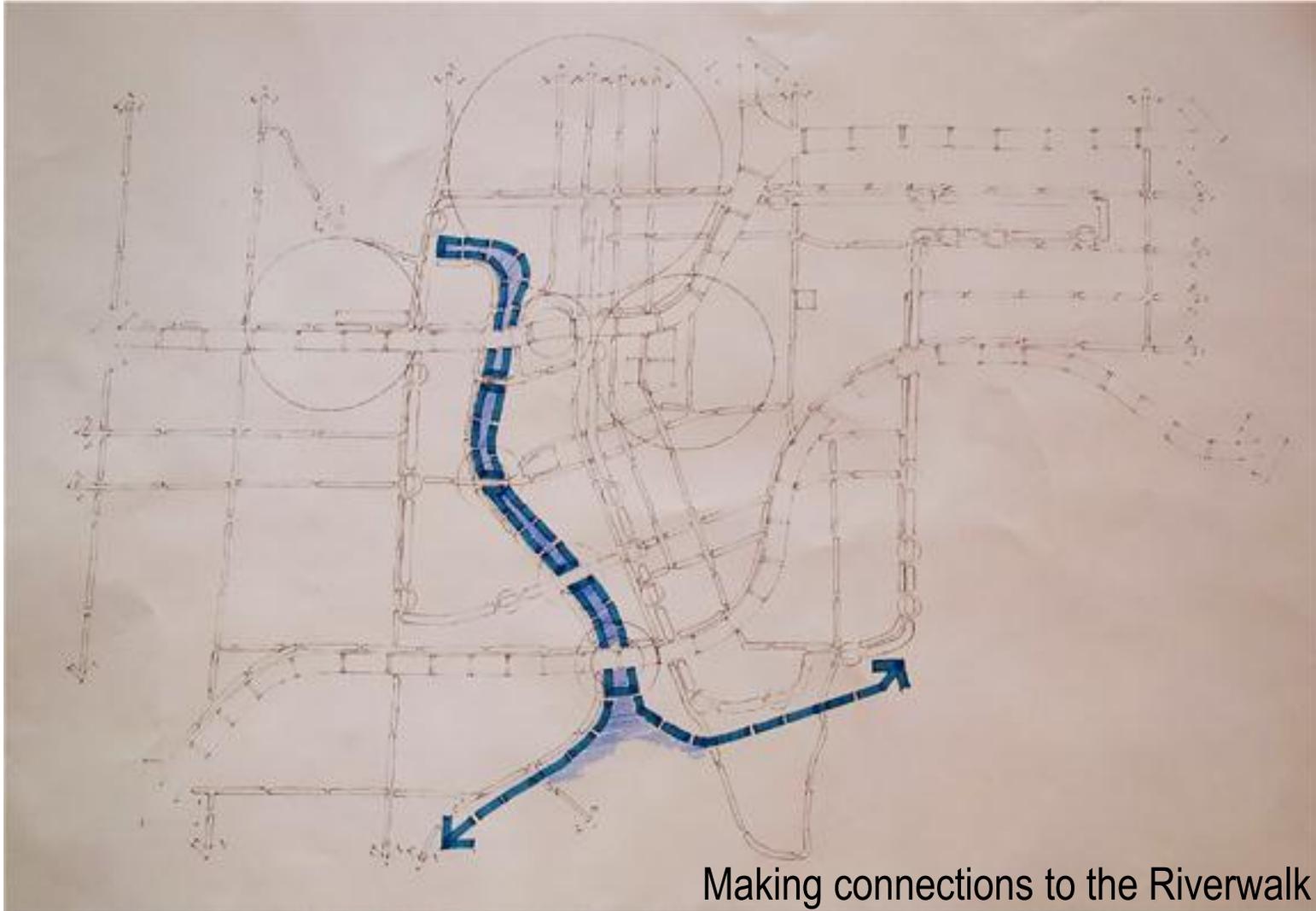


How can the city maximize connections to the Riverwalk?



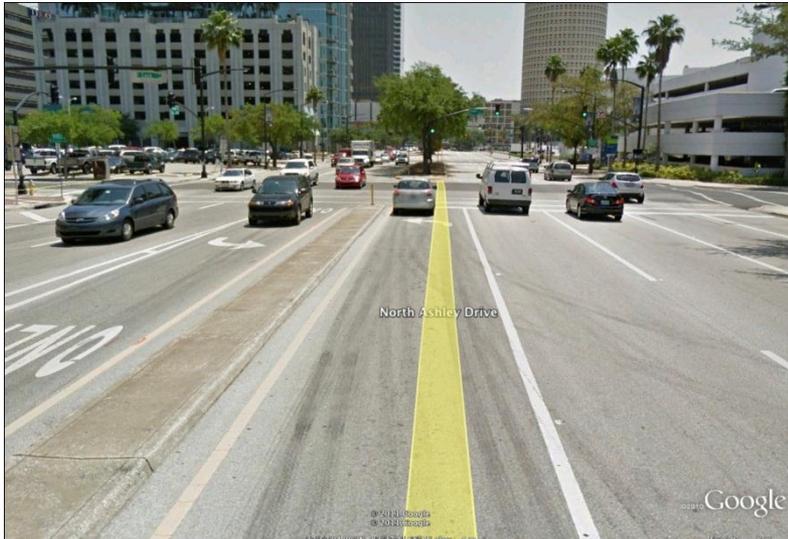
How can the city maximize connections to the Riverwalk?

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Making connections to the Riverwalk

Traffic Calming

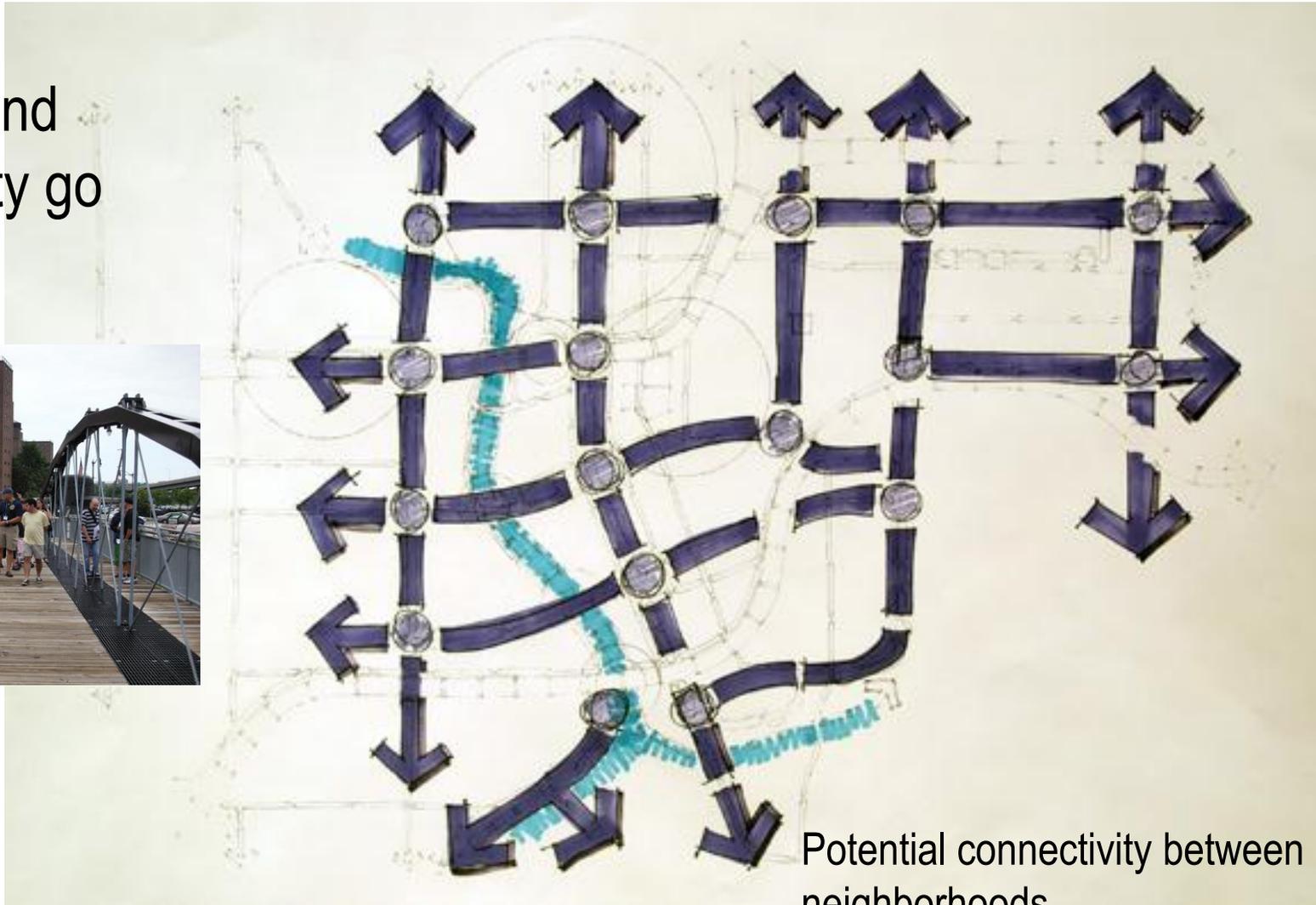


Ashley Drive Before



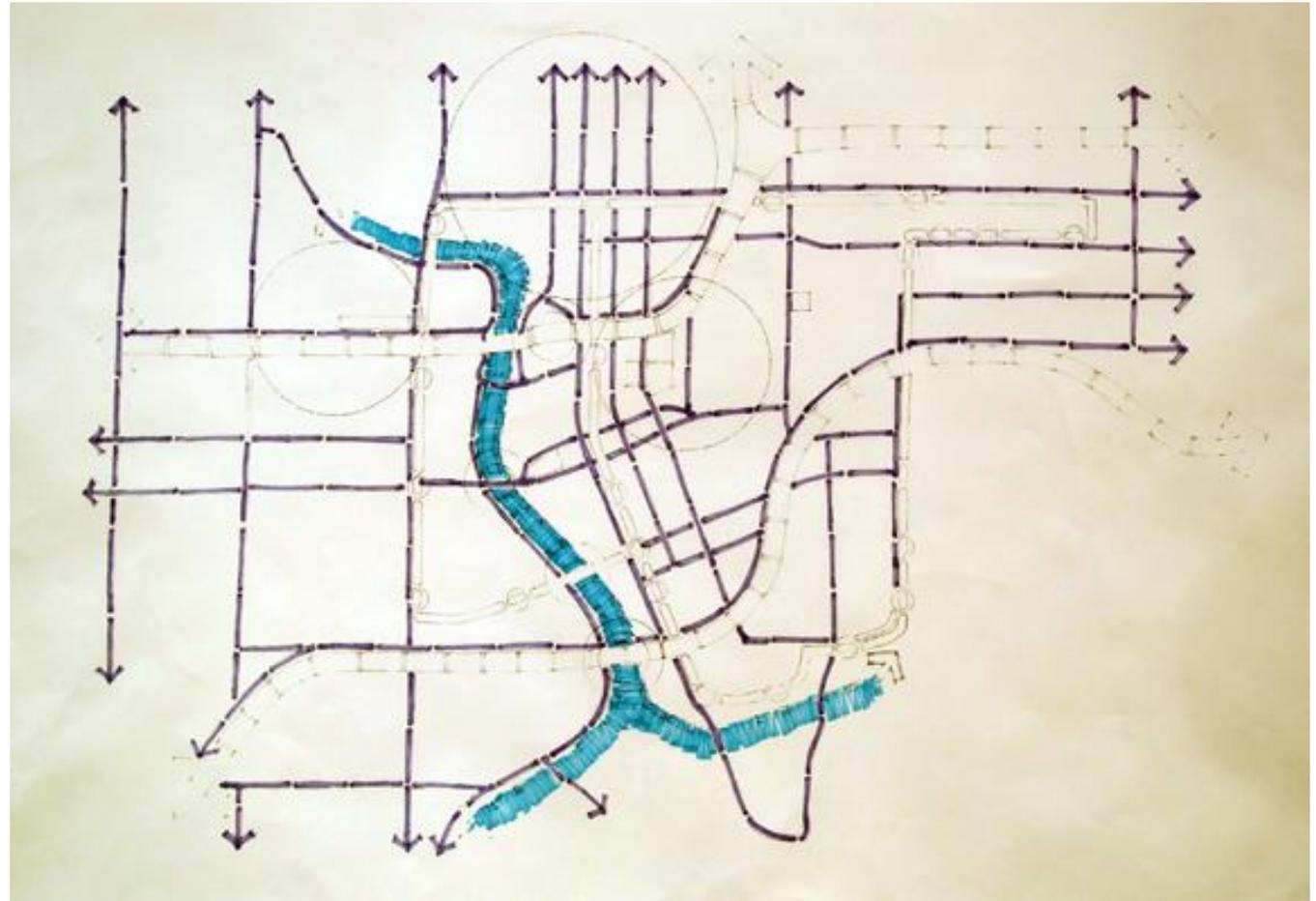
Potential for Ashley Drive

Livability and
connectivity go
together

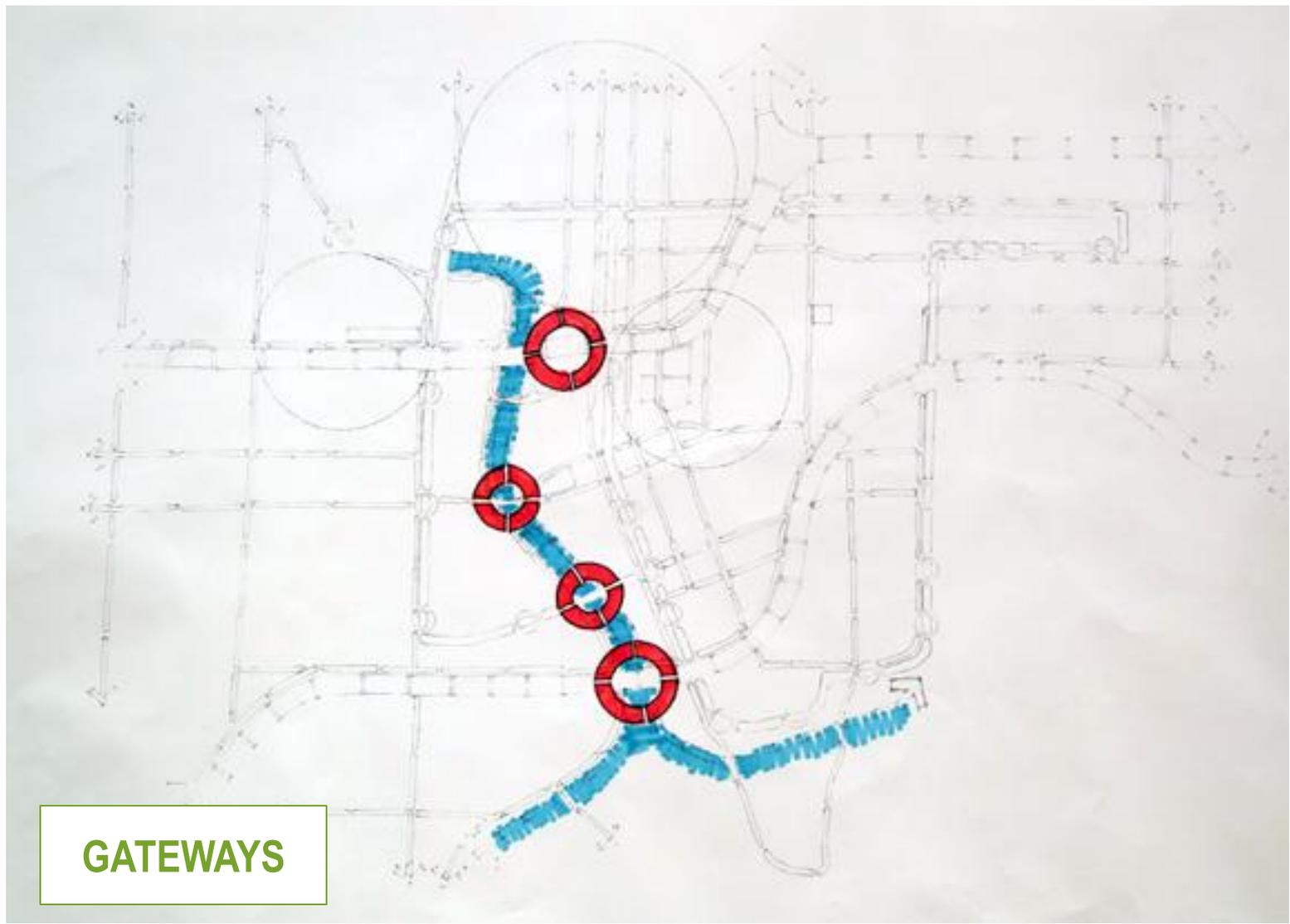


Potential connectivity between
neighborhoods

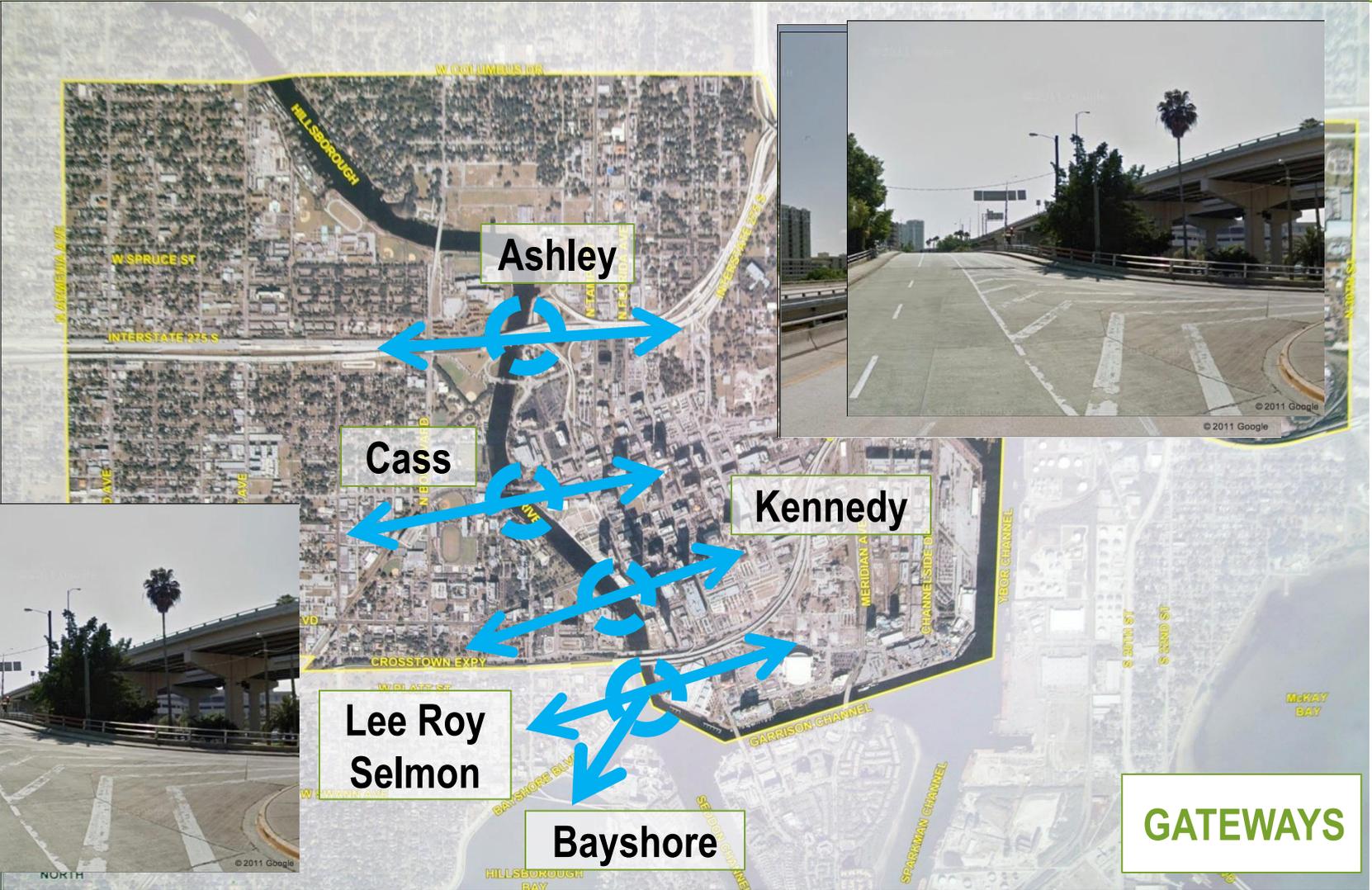
Livability and connectivity go together



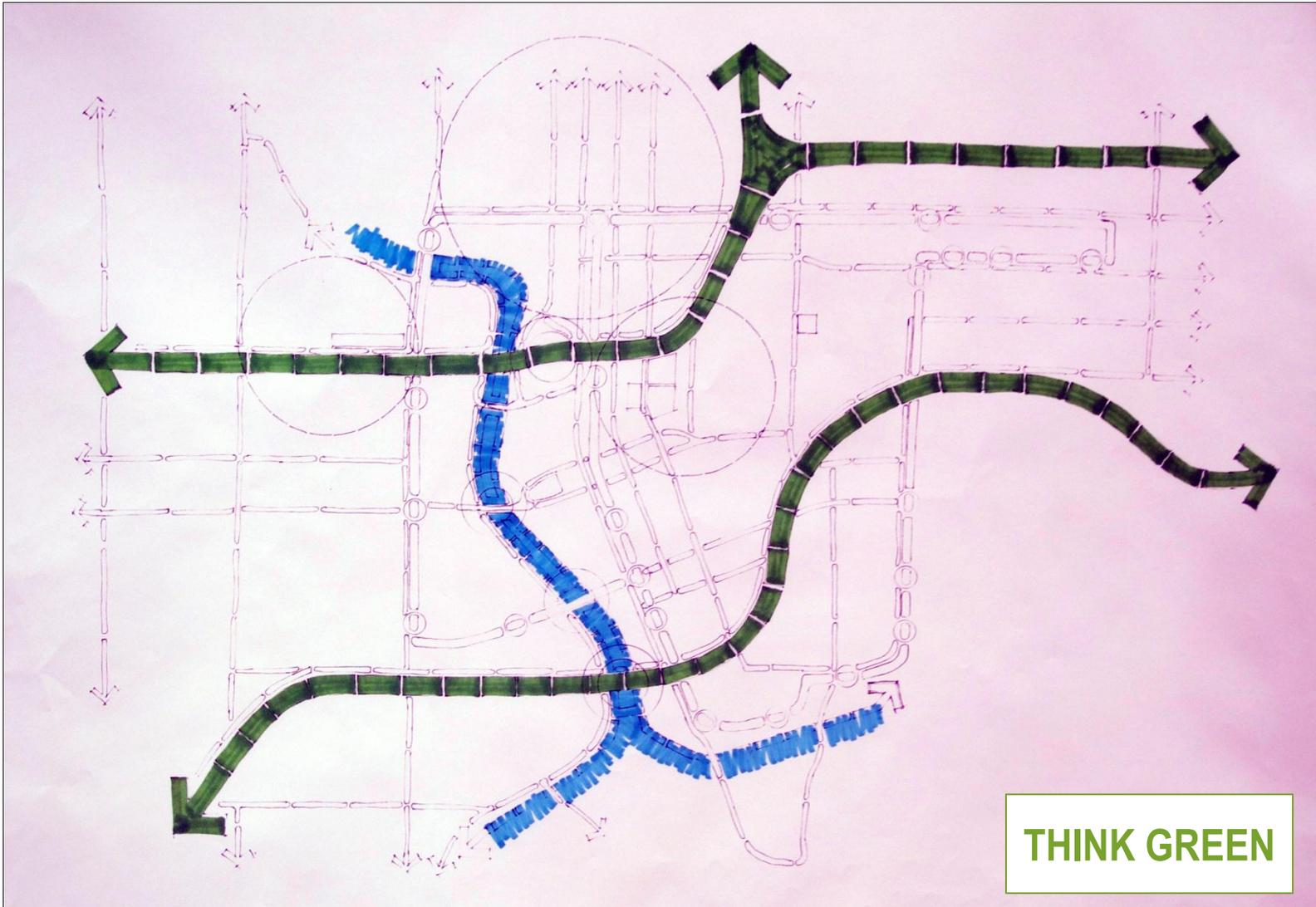
Potential bike lanes



GATEWAYS



How can the city maximize connections to the Riverwalk?



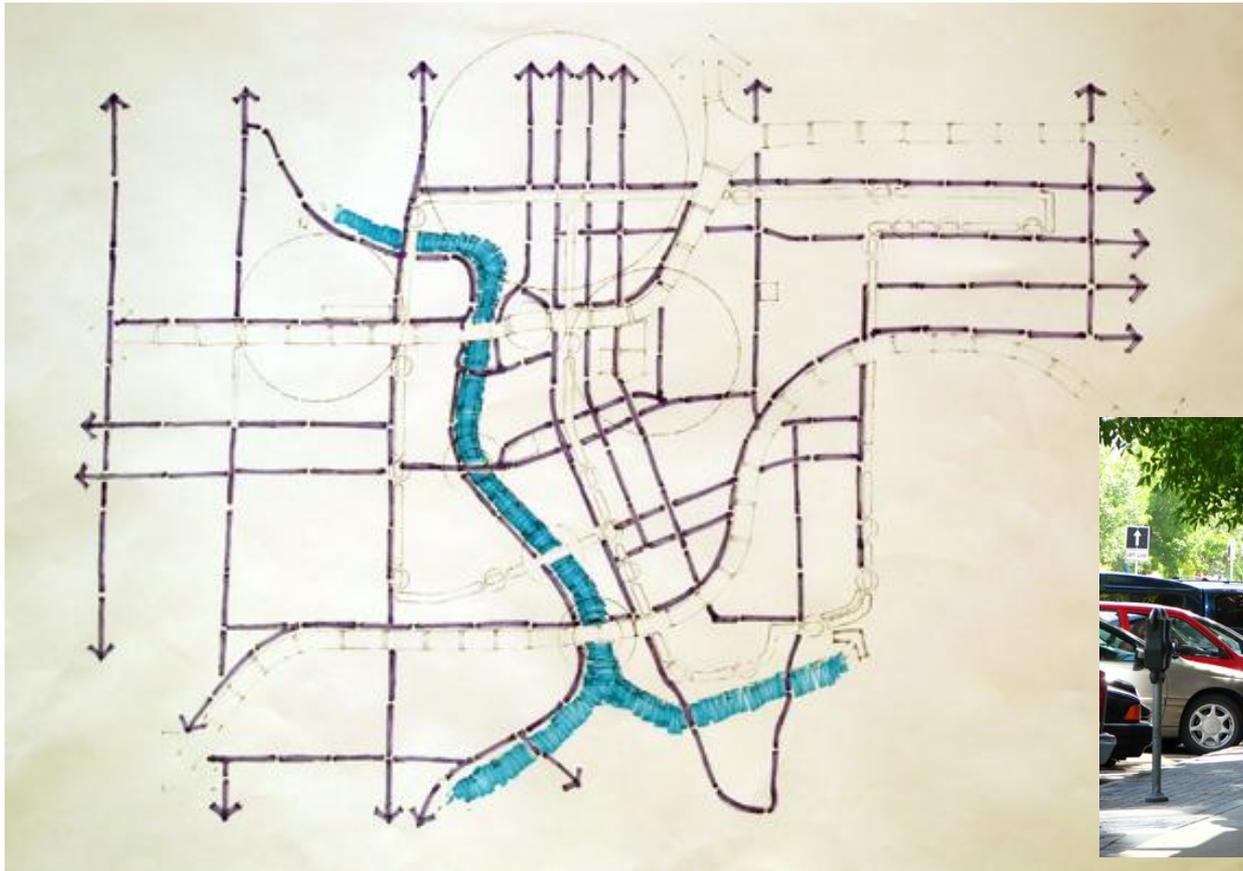
THINK GREEN

How can the city maximize connections to the Riverwalk?

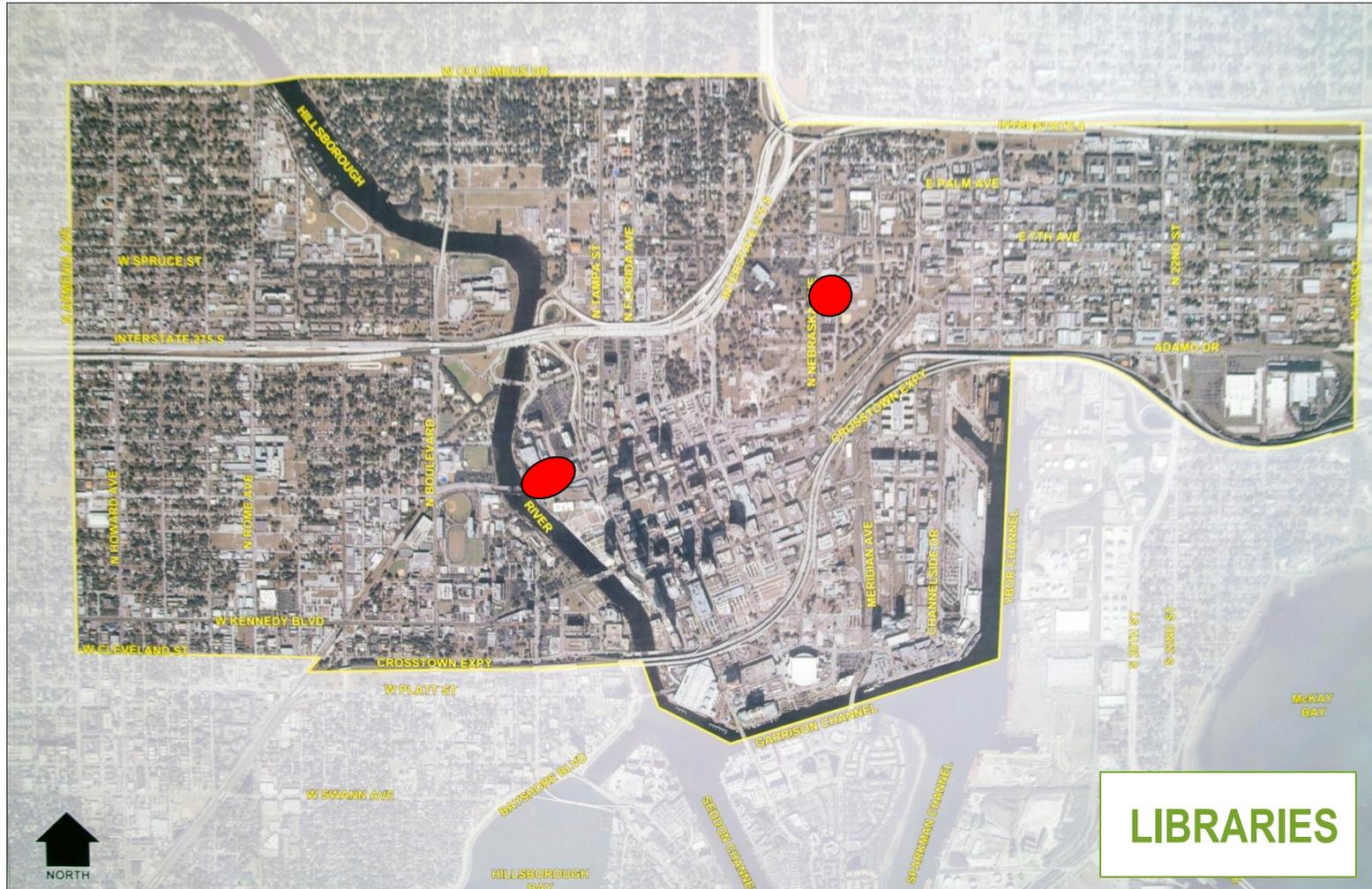


THINK GREENER

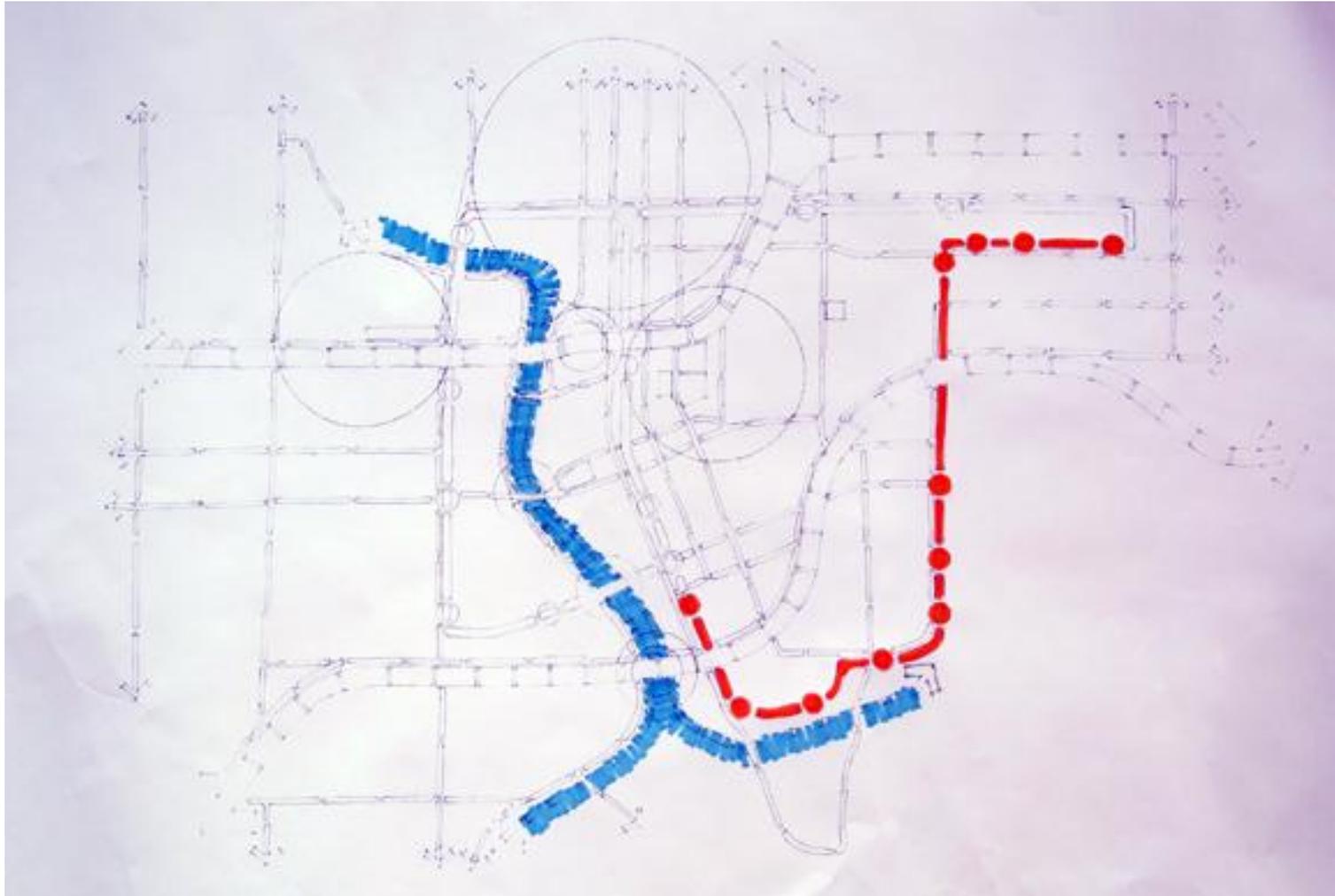
The urban experience is best defined by pedestrians.



Amenities already serve as connectors to neighborhoods

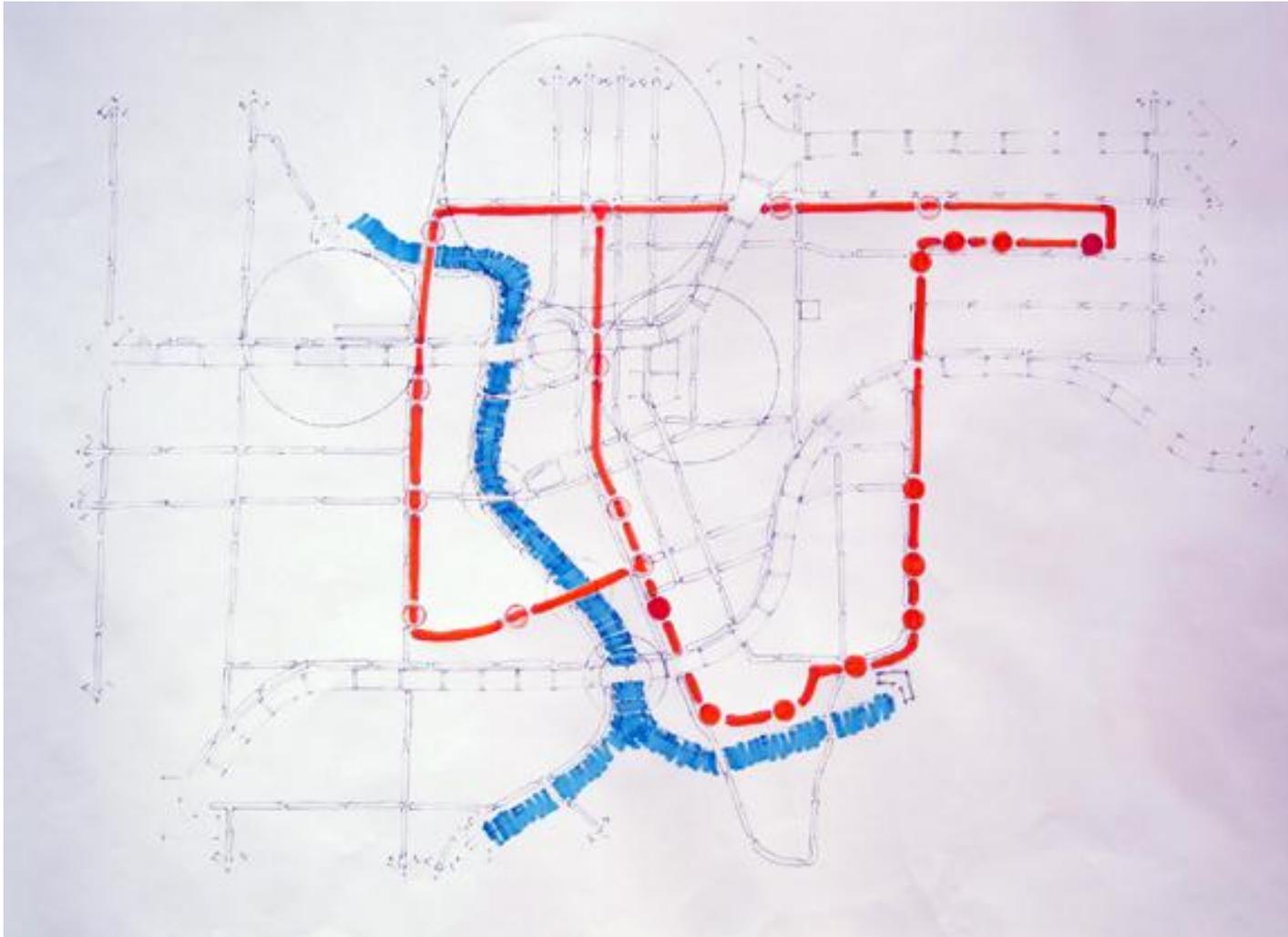


What can be done to strengthen connections between neighborhoods?



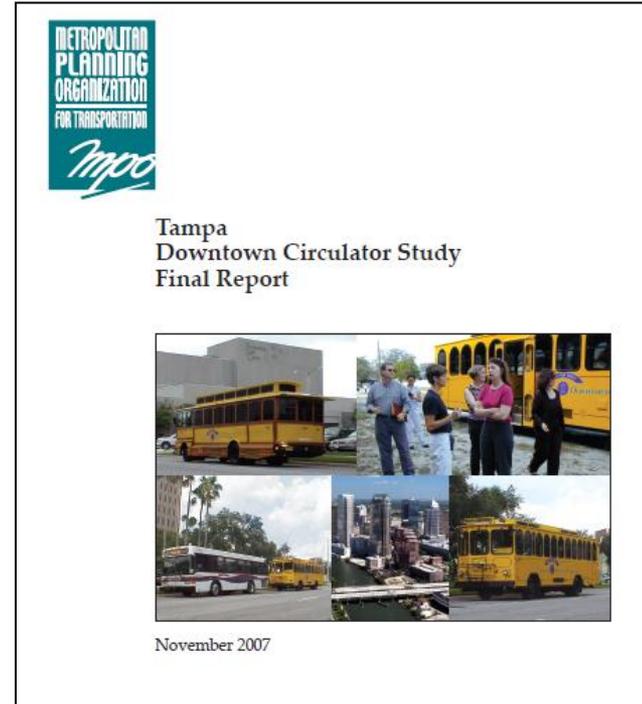
Existing trolley route

What can be done to strengthen connections between neighborhoods?



Potential trolley route

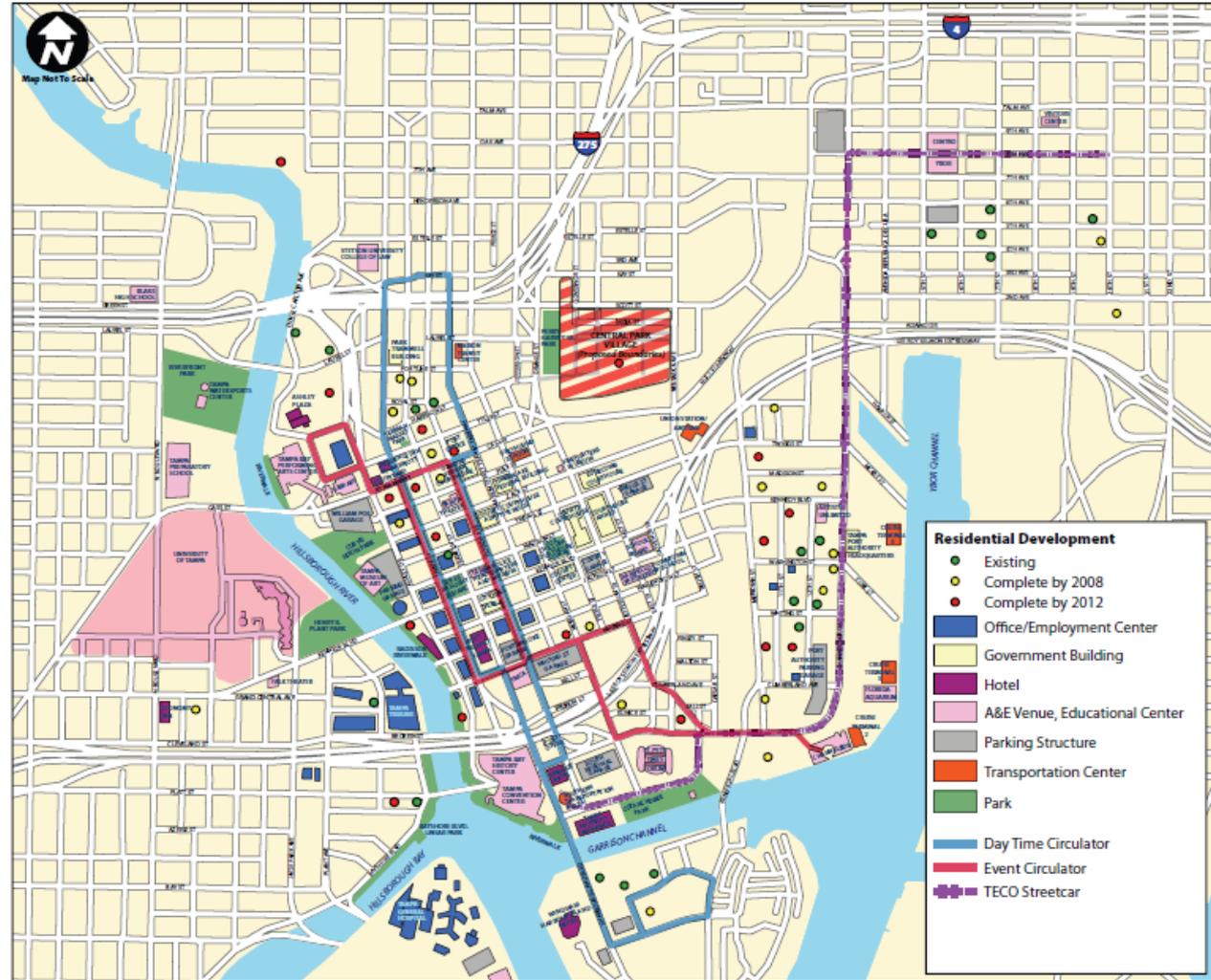
What type of circulation system should the city consider to improve connectivity within the study area?



What type of circulation systems should the city consider and pursue to improve connectivity and linkage between the Downtown and adjacent neighborhoods within the study area?

What type of circulation system should the city consider to improve connectivity within the study area?

Recommended routes from the Tampa Downtown Circulator Study



What type of circulation system should the city consider to improve connectivity within the study area?

The DC Circulator, with its distinctive, low-floored buses and innovative marketing, could serve as a model.

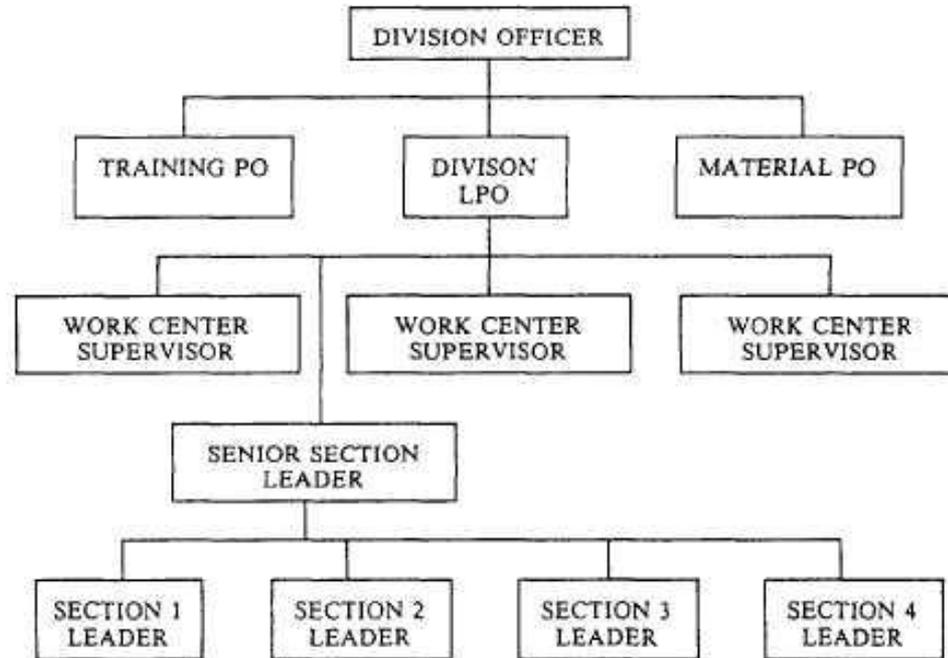


What additional areas of study should be included in the HUD grant scope?

- Additional items for study
- Sustainability/energy demand management



Based on a review of the approved scope of work for the HUD Challenge Grant project, what tasks or areas for additional study, design, or technical examination should be included in that scope?



What are some ideal organizational structures, including and public/private partnerships that the city should consider in pursuing sustained redevelopment within the study area?

Centralize Planning

- Centralize its planning and activities
- Strengthen its one-stop development shop
- Encourage the creation of a private, non-profit development corporation



Community Plans

- Community Plans are key to sustainable, efficient and predictable development
- Community Plans should engage all relevant city departments and neighborhood stakeholders to collaboratively develop a vision for the community



Development Service Office

- Centralized one-stop development service center will improve development services for everyone
- Development Service Coordinators
 - assist with code and policy compliance
 - resolve any conflicts that may arise along the way



Land Development Committee

- Staffed by senior managers with decision-making authority.
- Meet regularly to ensure disputes are resolved quickly
- Guided by newly created Community Plans
- Ensure all development decisions are made in accordance with good planning principles



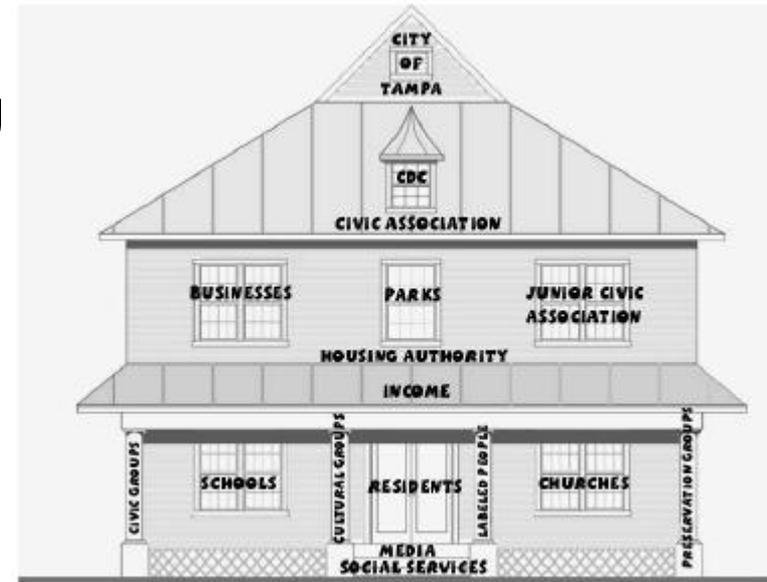
Community Partners

- CDCs and CDFIs can partner with the City to help achieve sustainable, thriving neighborhoods and a revitalized central business district
- Operate with a public mission working with local government, private sector and the philanthropic community to bring people and resources together to develop neighborhood centered development , businesses and other community-related programs and activities
- Community Development Financial Institutions promote economic development in struggling areas that are underserved by traditional financial institutions.



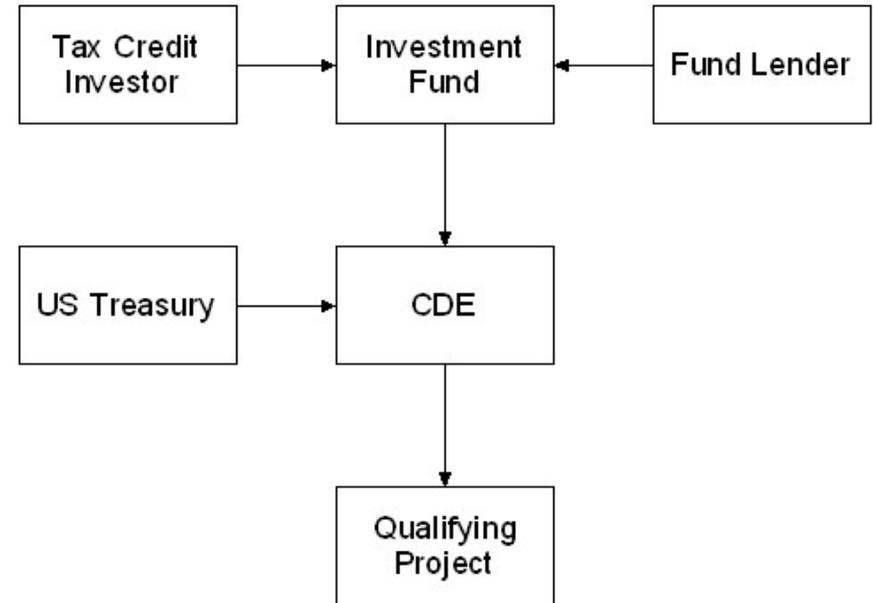
Community Partners

- CDFIs provide financial services including:
 - Mortgage financing for homebuyers
 - Financing for the rehabilitation of rental housing
 - Financing for the building and rehabilitation of community facilities
 - Commercial loans to small and microenterprise businesses
 - Financial services needed by low-income households and businesses in the target areas
- Community Development Banks,
- Community Development Credit Union
- Community Development Loan Funds
- Community Development Venture Capital Funds
- Microenterprise Loan Funds



What types of financing options are available to fund redevelopment in the study area?

- Housing Finance
- TIF
- New Markets Tax Credits
- Private Authority Bonds
- Operational adjustments to parking



What types of financing options are available to fund redevelopment in the study area?

Local institutions

- University of Tampa
- University of South Florida
- St. Leo's University
- Tampa Prep
- Stetson Law School



Regional Partners

- Hillsborough County
- St. Petersburg
- Hillsborough City/County Planning Commission
- MPO



Convene a
Kitchen Cabinet



Create a centralized planning and zoning system/structure to streamline and better coordinate the processes and to better ensure conformance with the vision and the master plan for downtown and the community plans for each of the neighborhoods that will be developed.

Develop, with citizen participation, a clear vision of Downtown Tampa and its relationship and connection to the near-downtown neighborhoods and have it formally adopt those plans.

Immediately enhance the landscaping of gateways
to the downtown and vacant potential
development parcels.

Complete the Riverwalk on both sides of the river, connecting to Bayshore and increase its utilization by connecting to the CBD and other transportation corridors safely

Reinforce the continuation/completion of projects already underway (Encore, Channel District), recognizing market conditions and making adjustments to current plans, if appropriate.

Focus new development activities on diverse housing offerings in and around the Tampa Heights/North Boulevard Homes/Marion Street Transit Station.

Engage the consultant under the HUD Sustainable Communities Challenge Grant to assist in these activities concurrent with the specific requirements of the grant and to provide detail analyses of funding and financing alternatives available to each activity.



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